

International

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Auster

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In this Issue . .

Volume 38, Number 2, June 2015

Hopefully you will have received this magazine about a week before the AGM and you are already preparing to go to Old Sarum to attend. It's a very nice part of the country and an excellent airfield so we should get a good turnout. Lets hope the weather is flyable. Almost half way through the year and it always seems to be miserable at the weekend, or is it just me?

It would appear that our begging for articles in every editorial has had its effect because we have received more than we can use in this edition. There is some very interesting international and local content and we would like to thank everyone for their efforts.

There were a few delivery problems with the last edition but hopefully these have been sorted out.

See you all at the AGM

Steve & Malcolm Isbister

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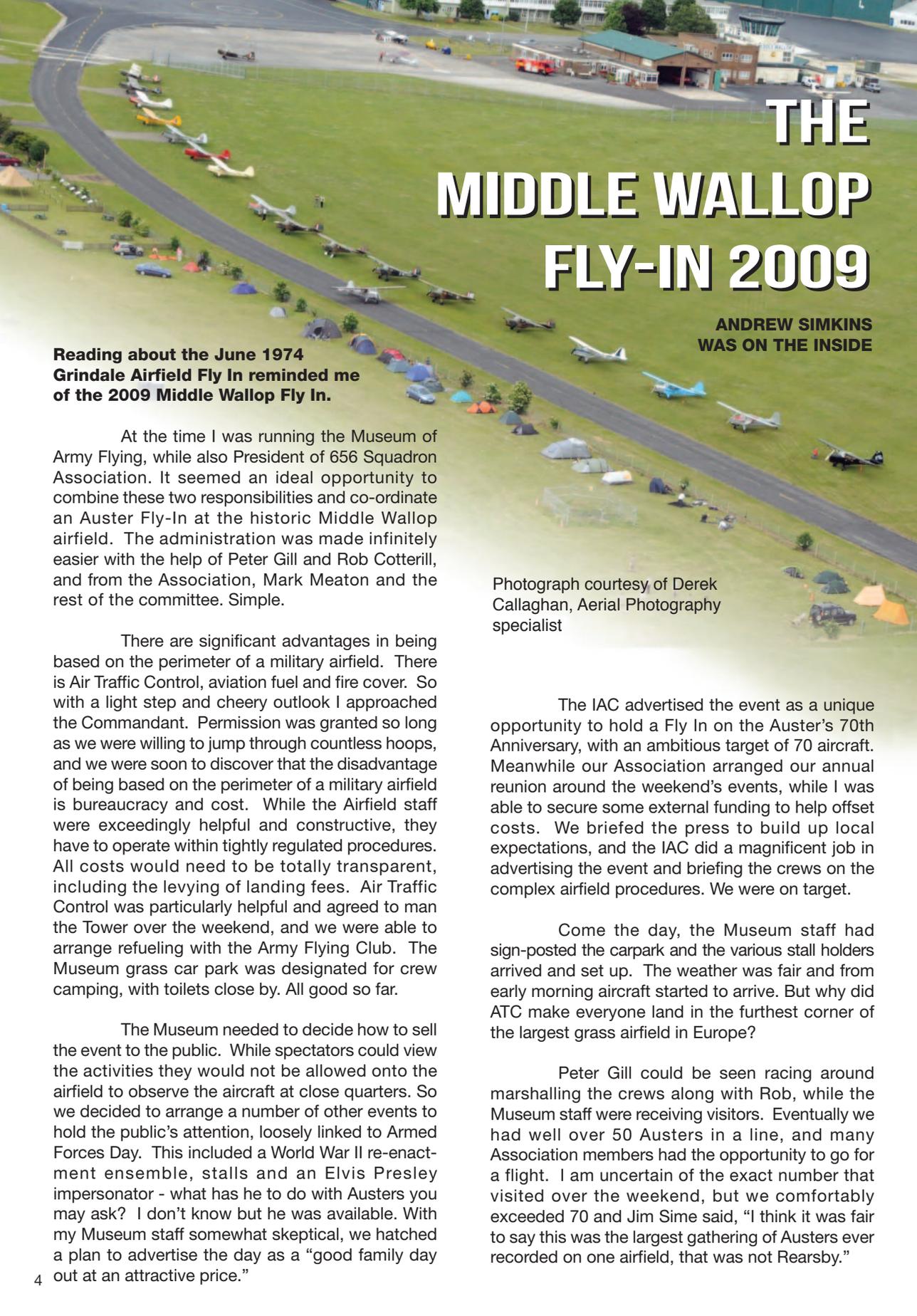
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Cover Photo: Paul Morton got this excellent line-up at Bidford



Clare May and Steve Farrant compare the 737 cockpit to the Auster
(Photo Mick Bajcar at Air Salvage Kemble)

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THE MIDDLE WALLOP FLY-IN 2009

**ANDREW SIMKINS
WAS ON THE INSIDE**

**Reading about the June 1974
Grindale Airfield Fly In reminded me
of the 2009 Middle Wallop Fly In.**

At the time I was running the Museum of Army Flying, while also President of 656 Squadron Association. It seemed an ideal opportunity to combine these two responsibilities and co-ordinate an Auster Fly-In at the historic Middle Wallop airfield. The administration was made infinitely easier with the help of Peter Gill and Rob Cotterill, and from the Association, Mark Meaton and the rest of the committee. Simple.

There are significant advantages in being based on the perimeter of a military airfield. There is Air Traffic Control, aviation fuel and fire cover. So with a light step and cheery outlook I approached the Commandant. Permission was granted so long as we were willing to jump through countless hoops, and we were soon to discover that the disadvantage of being based on the perimeter of a military airfield is bureaucracy and cost. While the Airfield staff were exceedingly helpful and constructive, they have to operate within tightly regulated procedures. All costs would need to be totally transparent, including the levying of landing fees. Air Traffic Control was particularly helpful and agreed to man the Tower over the weekend, and we were able to arrange refueling with the Army Flying Club. The Museum grass car park was designated for crew camping, with toilets close by. All good so far.

The Museum needed to decide how to sell the event to the public. While spectators could view the activities they would not be allowed onto the airfield to observe the aircraft at close quarters. So we decided to arrange a number of other events to hold the public's attention, loosely linked to Armed Forces Day. This included a World War II re-enactment ensemble, stalls and an Elvis Presley impersonator - what has he to do with Austers you may ask? I don't know but he was available. With my Museum staff somewhat skeptical, we hatched a plan to advertise the day as a "good family day out at an attractive price."

Photograph courtesy of Derek Callaghan, Aerial Photography specialist

The IAC advertised the event as a unique opportunity to hold a Fly In on the Auster's 70th Anniversary, with an ambitious target of 70 aircraft. Meanwhile our Association arranged our annual reunion around the weekend's events, while I was able to secure some external funding to help offset costs. We briefed the press to build up local expectations, and the IAC did a magnificent job in advertising the event and briefing the crews on the complex airfield procedures. We were on target.

Come the day, the Museum staff had sign-posted the carpark and the various stall holders arrived and set up. The weather was fair and from early morning aircraft started to arrive. But why did ATC make everyone land in the furthest corner of the largest grass airfield in Europe?

Peter Gill could be seen racing around marshalling the crews along with Rob, while the Museum staff were receiving visitors. Eventually we had well over 50 Austers in a line, and many Association members had the opportunity to go for a flight. I am uncertain of the exact number that visited over the weekend, but we comfortably exceeded 70 and Jim Sime said, "I think it was fair to say this was the largest gathering of Austers ever recorded on one airfield, that was not Rearsby."

In the evening our Association members held our annual dinner in the Museum, while the crews went down to the local pub. The next morning, the remaining aircraft dispersed and Museum opened as if nothing unusual had happened.

Would I do it again. Unlikely! However, the Museum made a profit and our Association members had an excellent day and the chance of a flight. I hope the IAC found it all worthwhile. In the wash up with the Middle Wallop staff, ATC agreed that their presence had slowed down activity, and in future it would be advisable to dispense with their services and rely on blind calls. The event reinforced the view that there are distinct advantages and many disadvantages of using a military airfield. It is a matter of whether the airfield has sufficient historic significance to justify the extra administration, cost and encumbrance.

The strangest moment for me was when I went into my office to find Elvis changing into another of his Rhinestone costumes. Then he left the building.

Andrew Simkins



Ted Maslen-Jones MC, DFC with Nigel Skinner at the Fly In. Ted is now 95 and still very active.

He was in 656 Air OP Squadron RAF and served in Burma alongside the 14th (Forgotten) Army. He is one of only two WWII officers to receive both the MC and the DFC.

EDITORS NOTE: John Bennett (Journal Editor, 656 Squadron Association) points out that in the March issue "reading list" on Page 9, the 'Fire by Order' should read by Ted MASLEN-JONES. Copies, signed by the author, can be obtained on the 656 Squadron Association website (www.656squadron.org) shop, where there is a wealth of Auster related info and photos.

Become a Friend!



The Society of Friends of the Museum of Army Flying (SOFMAF) is based adjacent to the Army Air Corps (AAC) airfield at Middle Wallop. With over 35 rotary and fixed wing aircraft on display, the Museum is dedicated to preserving the history of this fascinating strand of the British Army. SOFMAF is currently looking for new members to enjoy the many benefits, including free entry to the Museum, and to help support the future of the Museum.

The Army Air Corps has been based at Middle Wallop since 1957 and the Museum is not only the national Museum of the AAC but also holds the archives for the Army Air Corps.

By joining the Society of Friends, you get a whole host of benefits including free entry to the Museum for you and another adult, plus two under 16s during school holidays, as well as discounts on event tickets and shop purchases.

If you would be interested in joining the Society of Friends, please visit www.armyflying.com/friends to download an application form, or contact the Membership Secretary on somaffriends@outlook.com for an application form.

Membership is only £25 per year and provides the following benefits:

- Free entry for the member to the Museum.
- Free entry for an accompanying adult.
- Free entry for two under 16s during school holidays.
- 20% discount on event tickets.
- 20% discount in the Museum Shop.
- SOFMAF Newsletters.
- Opportunity to become involved with research, archiving and maintenance etc.

The Museum is open daily 10:00 – 4:30pm

Ron Neal's Technical Notes

AUSTER BRAKE SYSTEM ADJUSTMENT (CABLE BRAKES)

1. Jack the aircraft.
2. Check the parking brake is off.
3. Remove the vertical rectangular spring protecting clips from the brake back plates and insert the blade of a flat screwdriver in the exposed slot, so that it engages with the teeth of the internal star wheel.
4. Turn the star wheel one tooth at a time until the brake shoes are fully expanded and prevent the wheels from being rotated.
5. Unscrew the locknuts on the cable adjusters at the upper forward positions on each brake back plate and screw same out until there is no slack in the brake cables. Tighten the locknuts. (See also Note 1)
6. Turn the star wheels with the flat bladed screwdriver until both wheels are free to rotate. Refit spring protecting clips.
7. Remove aircraft from jacks.
8. Check brakes hold during engine run.

NOTE 1: After a period of time it may be found that the movement available on the cable adjuster at the upper forward brake back plate position is extended to its limits and no further adjustment is possible. If this condition exists the cable adjuster should be fully screwed in and the cable slack taken up by the adjustment at the brake heel pedal end of the cable.

- a) On early aircraft this will necessitate unclamping the brake cable on the forward side of the brake heel pedal, pulling the cable tight and re-clamping same.
- b) On later aircraft the cable slack can be taken up by the adjusters at the cabin end of the brake cable, which are accessible through the removable plywood panel on the pilots side of the cabin floor, immediately forward of the pilots seat.

NOTE 2: It may be noticed when rotating the wheel while adjusting the brakes that a degree of end float is apparent on the wheel and the wheel retaining nut is dead tight. This end float prevents binding of the ball races in the wheel bearings, but if this end float appears excessive feeler gauges should be inserted behind the wheel nut to check that a clearance of 0.005 inches to 0.012 inches exists. If the clearance is over limits, wheel shims may be fitted between the inboard face of the inboard wheel bearing and the shoulder on the axle.

NOTE 3: It may also be found that during rotation of the wheel when expanding the brake linings (item 4) that there appears to be high spots where the wheel will "grab". This is due to distortion of the steel brake liner as fitted to the actual wheel hub; the distortion being caused by corrosion "humps" between the aluminium hub and the steel liner. The only cure for this problem is to fit a new steel brake liner which will require the following: a) Remove the four bolts and tab washes retaining the steel liner to the hub. b) Remove the wheel bearings. c) Mount the wheel hub on a mandrel and using a lathe machine down the distorted steel liner to a thickness of approximately 0.012 inches when the liner can then be easily removed using the end of a flat bladed screw driver. d) The "humps" of corrosion in the aluminium hub can then be machined off, taking care not to machine any material off the basic internal diameter of the hub. e) The complete hub surface that will be in contact with the new liner should be treated against further corrosion. In the past we have used a SELENIUM ACID solution, but this may no longer be commercially available, however it is quite possible that a NON-CAUSTIC CHROMETCH PRIMER would be satisfactory. f) The new liner can then be installed after heating the hub on (for example) an electric hotplate and cooling the new liner in a domestic fridge. g) The four liner retaining bolts should then be reinstalled using new SP42-C tab washers.

Two Austers, one 4WD, and six adventurers take a six day tour of wineries, wilderness and mouth-watering menus. Kerry Skyring takes us on

The Great (little) Auster Gourmet Tour

The Great Auster Gourmet Tour was conceived on the return flight from the national fly-in at Swan Hill in 2005. Eleven hours of Auster flight in 2 days had seen my biceps grow at the same rate as my blisters. Biceps from flying Austers? It's the result of grabbing the overhead tubing and regularly lifting ones butt from the seat to ease the pain and friction from the seat tubing during long cross countries. Kim (youngest daughter) and I decided there must be an alternative to the long trip south and that it should involve flying, food and wine. Yes, our excursions to the AAAA event usually involve all three but we would change the ratios i.e. less flying, more food and better wine.



KAL seeks shade at McIntyre's Strip near Dalby

Day 1. Caboolture to MacIntyres field near Dalby

“climbing like a clipped kangaroo”

The itinerary was developed after it dawned on us that the area in which I had grown up and which we all knew well (south-east Queensland) had developed a remarkable resemblance to the south of France. Yes we were squinting into the sun at the time and there are no Gaulloise, or 2CV's and very few Roman aqueducts. Amazing what a tourist brochure can do. We plotted via email and came up with a tour that took us into fields familiar and new - and a not insignificant number of cellars.

Tour participants:

Kim Skyring - Chief pilot and general memory checker.

Rob Scott - Chief engineer and Gipsy starter.

Kerry Skyring - Second pilot, sommelier and would-be gourmand.

Genevieve Johnson - wardrobe adviser and media manager (still waiting for the call from Hello magazine).

Clair Skyring - Tour photographer and ground crew.

Nat Skyring - Land Auster pilot in charge of hyperbole busting.

The family J5B (VH-KAL) had been flown in to Caboolture from Hervey Bay by Kim while J1B VH-KBE was already based there. Kim and Rob were to fly KBE and I was to fly KAL with Genie as co while Clair and Nat would follow up in the Land Auster (Mitsubishi 4WD). It will raise no eyebrows among Auster flyers to know the Land Auster sometimes finished ahead of the Air Austers. And so it was on day 1. By the time the machines had been washed, loaded, coaxed into fossil-fuel burning life and urged to the end of Caboolture's east facing strip the ground crew was well into the journey westward over the Great Dividing Range towards the Darling Downs and our destination - MacIntyres airstrip - one of the great gliding fields of SE Qld. Genie and I were off first in KAL because we expected KBE to outperform us in both climb and cruise but shortly after take-off Kim called on the radio to advise that I should throttle back. KBE was climbing like a clipped kangaroo. A cruise prop, full fuel and generous baggage allowance had allowed them a good look at the power lines off the end of the strip.



KBE parked in front of the menu board for the Cellar Door at Jimbour House. No dedicated Auster parking but hey...

A fertile plateau, which can create and destroy fortunes as quickly as a summer storm, can flip your Auster on its back

Finally, as the Glass House Mountains slipped past our starboard struts we settled into a loose formation climb to 6,000 ft to cross the great divide, leaving behind the coastal green for the wide brown Downs – a place of rich soil and irregular rain. This is a fertile plateau which can create and destroy fortunes as quickly as a summer storm can flip your Auster on its back. But we were headed for one of the fortunes – a French Chalet fantasy known as Jimbour Station - built in the 1870's when an ounce of wool from a Merino sheep was approaching the price of an ounce of gold.

Clair and Nat were waiting as we landed at a deserted MacIntyre's strip. Rob immediately lapsed into animated story telling as he recalled happy days flying gliders in this thermal rich region. On with the trip! Clair jumps into the back of KBE, Nat into KAL's and 25 minutes later we land the whole gang on the sealed strip at historic Jimbour House, taxi into the restaurant's combined plane and car-park (see photo) do the wine tasting (pilots not permitted to swallow) do lunch, load the wine, fly back to MacIntyres and then drive to our overnight digs at Rangemore Estate, a B & B with winery in the foothills of the Bunya Mountains. Our accommodation is an old Queenslander style cottage, moved on to the property for guest accommodation. We enjoy the sunset as it draws the light from the green slopes of the mountains while we sit on the verandas of the "Cellar Door," a building which formerly housed the Fire Brigade headquarters of the nearby town of Dalby. Queensland buildings seem to lend themselves to pulling up stumps and moving on.

Day 2-3 Dalby to Stanthorpe

..a rich Auster past recalled by an AOP.9 and AOP.3 in the excellent museum..

This "gourmet tour" would not have been possible without the Land Auster to run us between airfields and accommodation. And in keeping with the original plan of limiting each days flight to the equivalent of a few hours on the road, next morning we point our props southwards for a 70 min.

flight (3.5 hour drive for Nat and Clair) to Stanthorpe, town of apples, apricots and, fancy that, grapes. The first section of the route leads over an unrelentingly brown and drought afflicted landscape. Off to the east is the Oakey Army Aviation Centre where army pilots learn to fly helicopters. It has a rich Auster past which is recalled by an AOP.9 and AOP.3 in the excellent museum.

The town of Warwick is also east of track and disappears over our shoulders as Stanthorpe's green and forested surrounds come into view - a welcome break from the sun-burnt farm fields of the Downs. We put the Austers down on (yet another) deserted airfield and by the time we have secured and unloaded them the Land Auster is pulling up at the airport gate. In Stanthorpe we've lined up a winery tour and rented a house for two nights so that all crew are under one roof. The kitchen becomes the centre of gourmet ambitions as the would-be master chefs compete for accolades. Kim's breakfast pizza scores well.

Day 4-5 Stanthorpe - South Grafton - Sandon River

The log book shows 1hr 15mins from Stanthorpe to South Grafton and a further 20 minutes to Sandon River but it doesn't reflect the sudden nostalgia attack I suffer as we skim over the rugged eastern escarpment of the Great Dividing Range heading south-east into the upper reaches of the Clarence Valley. I learnt to fly at South Grafton, a small grass strip on the edge of the town, more than three decades earlier. It must be approaching 20 years since I last landed KAL here. Each bend in the river below is familiar and I wonder how I will feel about this home-coming after all these years. Overhead the field - there's Susan Island in the middle of the broad river and the strange road-over-railway bridge - a left turn over the town, downwind across the horse racing track and then turning base and final for 08, touching down on a

strip of unfamiliar tarmac which has replaced the grass of my student pilot days. It feels good coming back to the nest from which I first flew. One or two locals remember the Gipsy's sound and come out to say g'day.

Genie had taken the land Auster for this leg with Clair joining me in KAL because her first flights were from the South Grafton strip. Soon Nat and Genie join us and we head to the supermarket to stock up on gourmet groceries for the next leg of the tour and then to lunch at the Crown Hotel, right at the end of the main street and overlooking that wonderful stretch of river where, tired at the end of its meandering mostly southerly route from the high country, it makes a dramatic turn to port, relaxes its hold on the gathered waters and begins a slow spilling into the Pacific's endless swells.



Sundowners at Tooumbal House

*“it makes you want to kiss the ground
the air and the sea”*

Lunched, we launch ourselves to the final destination, Tooumbal Plains House in the Yuraygir National Park. With an airstrip leading to the front door it will be much easier to for the air Austers to reach than the land Auster. Nat will spend some time, and exploit the 4WD, as she tackles sandy tracks on her way into the house. This northern rivers region of NSW is an achingly beautiful part of the country with broad rivers, green fields and beaches that make you want to swim, fish, run and fly. Our rented house has no neighbours within sight and is just a short walk from the beach. It is an award winning piece of architecture, built in four blocks around a central courtyard and with walls that slide back to reveal the rolling countryside complete with grazing kangaroos and strutting emus. Bliss. We settle in, light the fire, and the cooking begins: prime steak, fresh local prawns, oysters - and of course it's all washed down with fine Stanthorpe wines. But first I am compelled to take KAL for a run along those beaches that I have not seen for so many years.

So much beauty, so much freedom! It makes you want to kiss the ground the air and the sea. The log book shows 45 minutes of local flying but mentions nothing of the pilot's emotional state.

Day 6

Sandon River – Caboolture – Hervey Bay

*..something on the radio about
50 year-old spars*

Our two days at Tooumbal Plains House, Sandon River, are not nearly enough but time and money are running low. The land Auster is packed and dispatched, KAL and KBE are swung into smoky life and we head north, inland of the Evans Head RAAF range, a little east of Lismore and then into severe turbulence over the hills south of



Auster and bath

Murwillumbah. Rob says something on the radio about 50 year-old spars. We throttle back and ride it out. Genie is stoic. Our track is then east for the coast south of Coolangatta before following that breathtaking route along the Gold Coast beaches, waving to the guests in the high-rise apartments as we scoot past below their top-deck pool bars. I get the feeling they want us to envy them. I'd sooner be flying. Then it's over to and along South Stradbroke and North Stradbroke Islands, Moreton Island – breathing in while we cross the bay to Bribie Island - and on into Caboolture where our tour began. That leg, one of the most scenic one could ever fly, was 2hr 35mins. Genie and I leave the rest of the crew here, refuel KAL and continue on to Hervey Bay, another 1hr 35mins.

The point of me scratching around in the memory coop to tell this story is to a. give myself a kick-in-the-butt to get KAL's refurbishment finished so that I can go and do it all again; and b. hopefully inspire a few Auster owners to get out the WAC's, get mates and spouses together and go touring. So many places, so little time, and no time like tomorrow.

Words and photos: Kerry Skyring

PLANES or DAMES

Ken Simmons has had a passion for all things aeronautical (including a few Austers) for most of his life.

It would have been around the year 1937 when I was woken by my father and half asleep rushed into the garden of our house in Worthing, Sussex. He was most anxious for me to see a most unusual machine in the sky.

I cannot recall any names or markings; it was quite late but we could clearly see the long cigar shape, silver in colour. The one thing I can remember is the deep hollow sounding engine, possibly two as it passed quite low over our house flying in a northerly direction (possibly Croydon).

Later that year, on a visit to Cowes on the Isle of Wight I spotted another aircraft unknown to me until I asked a mechanic working on the engine. He told me it was a 'Supermarine Walrus' – an odd looking aircraft which had a large engine mounted on top of the wing above the cabin and I believe this had a pusher propeller. The mechanic told me there was a similar aircraft called 'Sea Otter' which had a conventional layout.

I think I was hooked right then and can say with certainty that I never found the time to have any interest (unlike my contemporaries) in the opposite sex.

The war came in 1939 and we saw many English and German fighter planes engaged in dog fights over our school.

It was probably during 1941/2, together with around eleven other interested lads that we formed the 'Horsham Model Aircraft Club'. We made and flew gliders and rubber powered aircraft. My own special interest was control line aircraft where control was obtained by two wires running from a bell crank on the model, to a handle in the operator's hand giving elevator control. The models were powered by wonderful small light diesel engines with some fitted with glow-plugs. Many is the time my finger received a whack from the propellers.

After my time there came the radio controlled models, I would have loved to have been involved with those. It was, however, an enjoyable time and each summer we gave demonstrations on Horsham Park on their open days.

At the age of twelve I joined the Air Training Corps as a cadet. My interest in aircraft was boosted when, at a later time, I was promoted to Warrant Officer responsible for teaching aircraft recognition. In those days there were noticeable differences in appearance and sound between English and German aircraft. Today, because of advances in design it is less easy to tell some apart. During my time in the cadet force I was lucky enough to fly in Tiger Moths, the standard aircraft used for giving cadets some flying experience.



An Auster V, a type Ken worked on

I remember being fortunate to also fly in Dakotas which were fitted with rollers along the floor of the cabin and also on hastings whilst on crew training in recovering from stalls etc.

I was also selected to join a flight in a Lincoln Bomber (RF 358) engaged in navigation exercises to Iceland – as a base then flying over the North Pole. This flight I seem to recall was one of twenty-three hours, flying up one side of Greenland and then making flight south. I was told the bomb bays had been fitted with extra fuel tanks for this trip.

During this period of time I left school and joined a local building company on a six-year apprenticeship as a carpenter and joiner. I was still in the cadet force at this time and was able to learn how to fly the 'Kirby Kadet' Glider. At that time you could only learn up to 'A' certificate level, which comprised of a tow up using an old barrage balloon winch, with one cadet standing by with an axe to cut the wire cable should it not release! Health and Safety would have had a field day but in any event I'm not sure it would have cut the cable anyway.

I joined the Royal Air Force in 1949 on a three-year contract. After basic training of some thirteen weeks I was accepted for training as an airframe mechanic. This was on a R.A.F base at St Athan in Wales. Basic training was on a variety of aircraft including Tiger Moths and Meteors. On completion of this training I was posted to 651 Air Observation Squadron. This was based at Castle Beniot near Tripoli – now called Idris Airport I believe.

This is when I was able to get hands-on experience working on Auster Aircraft. As I recall we had several Mark 6 Austers powered by the Gipsy Major 7 Engines producing 145 H.P, equal to the Gipsy Major 10/11. This model had the new moulded windscreen replacing the earlier flat screen.

We also had one Auster V powered by the Lycoming horizontally opposed engine. Neither of these aircraft had self-starters and because of the high temperatures did sometimes take quite a few swings before starting. The Mark V rotated opposite to the Mark 6 models and since I seemed to enjoy more success in starting the Mark V it was this aircraft I spent much of my time working on.

The registration number was TW448 and I loved working on her. A few years ago I heard it had been found and a group were trying to bring it back to this country and I am still trying to locate said machine.

The Austers were very robust and I encountered few real problems. I can't speak for the engines which were not part of my responsibility. The under carriage however was 'sprung' using an inch diameter rubber loop made up of 1/8 square elastic with a strong cotton covering. I am not sure what exactly happened but, I assume had not been released because while cutting through a defective loop, part of it swung out breaking most of my lower teeth! Another item that seemed to wear out quickly were the aileron hinge pins – possibly due to the abrasive effect of the sand. Much of the flying taking part from hastily made up desert airstrips.

We frequently went into the desert to locate suitable landing strips – by madly driving a ten tonne lorry over a potential site. If it appeared sound one of our team would stand at one end holding a white rag to indicate wind direction and strength. The object then was to see how proficient we were at disguising an aircraft on the ground by use of poles and betting which we carried with us at all times. If the Army pilots spotted us we would move along a little and try again until they were happy with our efforts. Sometimes it was necessary to change an engine away from base and we would rig up a tripod to lift the engine out and fit a replacement.

At some time in 1951 we were working out of Ismalia. It was a time of some unrest and I know our Austers were used as spotter planes, locating the resurgents who used to hide arms in the local cemeteries. We often had small arms fire whizzing over our living quarters at night.

During this period we took delivery of two packing cases containing an Auster Mark 6 TW636, but this one had a self-starter. Our crew assembled this and it joined the rest of the squadron. I submitted photographs of this and many other aircraft to the Museum of Army Flying at Middle Wallop together with details of times and places which are now on record and no doubt can be made available to interested parties.

I was posted to R.A.F Luqa at about this time and was pleased to join the Malta Communication Flight. This unit was responsible for flying Lord Louis Mountbatten together with other officers to NATO countries for their meetings. Lord Louis had a special Valetta Aircraft VX575 reserved for his trips and an Anson was also deployed on the unit. One of us would fly with the flight to carry out in-flight checks and refuelling.

Also whilst in Malta there were three Spitfires present for the filming of 'The Malta Story' although we had limited contact with these aircraft.

I had a lucky escape on the 15th September 1953 as due to space constraints I was unable to board the other Valetta we had in Malta VX562 which was returning to England. Early that morning in dreadful weather it collided with a Lancaster TX270 on an anti-submarine exercise and all 19 crew in the Valetta and seven crew in the Lancaster sadly lost their lives.

I cannot add much more to the foregoing but I have lost none of my passion for all things aeronautical. My son enjoys the same feeling and takes me to my local airport which is quite seasonal particularly on the commercial side.

On my 80th birthday my children paid for me to take a one hour flight in a microlight aircraft at Compton Abbas, possibly the most attractive of all private airports. This was a real pleasure and flying at 4,000ft I had stunning views of the Dorset countryside.

Ken Simmons

FOOTNOTE:

I feel certain that anyone reading these notes will know the following book title; however, I include it since the content makes good reading for anyone touched by the 'Auster Magic'.

The History of the Auster Aeroplane

by A.V. Hitchman

Printed by: Hounsfeld Design Ltd

9/11 Midland Road, Old Market, Bristol BS2 0JT



A round-up of pictures and reports of members' aircraft and their comings and goings

The club trip to Kemble on 25th April was a great success, good weather and an excellent guided tour around Air Salvage. Mick Bajcar got this shot of the Editorial Airedale RNP and Terry Lee's Terrier posing in front of a Boeing 737 awaiting scrapping. We thought it best not to leave RNP there for too long in case Air Salvage got the wrong idea!

Terry was also at the Fenland Vintage Aircraft Club April Fly-In and is seen here arriving.

Halfpenny Green hosted an Easter Wings & Wheels and Mick got this shot of Autocrat G-AJIH which came in from Long Marston. It bears a Latin motto on the nose "Ad Astra Per Alas Porci" - To the stars on the wings of a pig!

Paul Morton has been out and about and was at the Kemble (or should I say Cotswold) Bronco Fun day and Fly-In where Moira and Kevin Hale were flying the flag for the Club in Auster AOP-6 G-BNGE/TW536 seen here about to depart for The Abingdon Airshow the following day. Kevin was also at the fly-in at Bidford.



**Please send any contributions to:
editor@austerclub.org**

RESTORATION NEWS

Kingsland moves to Surrey



G-AJIT the well-known Kingsland Auster, suffered a mishap in the latter part of last year and was written off.

In a roundabout way, I have acquired the airframe – minus the engine, and intend to carry out a full restoration over the next few years, hopefully with an O-320 engine. I've never done this before and know nothing about welding, sheet metal work, electrics or trimming. In addition my woodwork is rubbish and I've never done covering or painting. So, piece of cake then? Gulp!!

A couple of weeks ago myself and Colin Harding hired a 20 foot van and had a long drive to Yorkshire and back to bring it home. I'm now having to find ingenious ways to store it all.

If anybody is able to offer any help or advice on any aspect of this, it would be much appreciated (my contact details are in the back of this magazine – see Treasurer). Also if anyone knows of an O-320 engine or the horn balance type of fin and rudder for sale I would be interested.

Steve Farrant



The Auster J/1 Autocrat SE-CGR built in 1946

For 4 years, 6 friends (mechanics and pilots) have kept a Boeing Stearman airworthy. We were looking for an Auster to pursue an affordable and rewarding project for all of us.

In order to have an structured organisation that would allow us to undertake the restoration of a new aircraft, we decided to create an association " "Les Ailes D'Autrefois (Wings from past) was born in April 2014, in the southwest of France, about twenty kilometers west of Toulouse.

Our research led us to finally meet a Swedish owner of a fine collection. At the end of 2014 we acquired Auster J/1 SE-CGR at the Museum of Cars and Aerospace Svedinos, Ugglarp county, on the south west coast of Sweden, halfway between Malmö and Gothenburg.

Since then fifteen people have joined us and we are going through the process of getting the Auster airworthy.

The initial registration was G-AIZW and it has not flown since 1966. A thorough restoration of all of the aircraft is required. So thank you in advance to anyone who will kindly answer any questions we put on the forum and help us in our desire to see this Auster fly again soon.

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Roger Lane brings us up to date on his **AUSTER J/4 RESTORATION**

This article was supposed to follow on from Part 1 which appeared in the December 2013 edition, but like many people, finding the time and I guess the courage to start writing something has been a challenge, so sorry again Malcolm et al for the delay.

So now I have some time I will try to sum up the last 6 years. I found my J/4 by accident, it was mid-summer and I decided to google 'Auster for Sale', and sure enough several popped up including G-AIJS. I had always loved the idea of finding a wreck in a barn and bringing it back to life, and that's basically what happened. By chance the next week I happened to find myself in the UK (I have lived in France since 1994) and I went to see IJS in Hatfield where it was being stored in a garage, and later that day drove to an IAC fly-in at Tibernham to find out more about Austers. What a day that was for my son and me, in my mind I had decided to buy the aircraft and this decision was sealed by a warm and friendly reception at Tibernham especially by Peter Gill, Stuart Allan and Al and Dot Mathie, and then 2 flights in different Austers, and to finish Steak and Chips in the hotel with a couple of pints of Greene King, doesn't get much better than that!

I finalised the deal on G-AIJS in a pub near Heathrow a week later and the barman, who I knew, found it most amusing that someone had bought a plane in his pub. Several more pints to celebrate this time London Pride, notice a theme yet? Then the previous owner Chris asked, "when can you pick it up"? Good question I said, having never moved something that shape and size, I had no idea how to proceed. I got quotes from several removal companies, one quote nearly being the same price as the Auster! Luckily a friend of a friend runs a truck company near Calais and came up with a great price, and a month later to the surprise of the neighbours and friends in France there was an Auster sitting in my driveway.

Before putting G-AIJS in the garage, I took the opportunity to put it together in the front yard, as I had only seen it with wings and tail feathers off and this would be a good experience for the future when the time would come to rebuild it. Years of Airfix training as a nipper made this task seem



Assembled on the drive and almost complete for the first time in nearly 40 years!

easy as did the fact there were holes in the wings and fuselage and the odd extra tear and scratch didn't matter. I will obviously be having kittens when I reassemble it when everything is nicely covered in new ceconite. Feeling dead chuffed to have a whole aircraft as a gate guard we cracked open a bottle of Champagne –see I don't just drink beer!

Then the question was where to start. I had planned to re-register the aircraft in the Fox Papa category which is the French homebuilt license similar to the UK LAA Permit. I sent in the paperwork and reserved the registration F-PIJS, I then expected an inspector to come and have a chat but was told this happens only when the aircraft is assembled and ready to be recovered. Luckily I had joined the Auster Club and was making many good contacts and asking lots of questions to enable me to plan the dismantling and rebuild. I also started collecting all the Auster manuals and old documents that I could find.

I decided I would split the rebuild into 3 phases: Fuselage, Engine and Wings. The aircraft had not flown since 1971 and the wings were not the original ones but had come from a French J/2 Arrow. The fuselage and wings had been kept in dry conditions but 40 years of sitting idle had made its mark. The frame had mainly light surface corrosion on it which was easily removed but the wooden formers and cockpit section were rotten/split in many places so they were all replaced with new spruce using the old pieces as templates. I decided to look inside the engine whilst it was in situ and after a week long battle to loosen the main nut that holds the prop hub on I was able to see inside the block and the horrible black mess inside. The engine had not been inhibited and 40 year old oil was encrusted on everything and a congealed lump of sludge lay in the bottom of the oil tank and filter. I later discovered that the moisture had separated from the oil and had sat on the bottom of the oil

tank, the result of which was a long strip of rusty metal that turned out to be unrepairable, so the tank is being now being completely rebuilt.

It was a year or so later when I stripped the motor to find that one of the big end bearings was badly cracked (see photo) and I guessed I had found out why the aircraft had stopped flying in 1971.

Anyway, time passed by quickly, the film "The Time Machine" springs to mind where the years roll by like a slot machine. I tried to do a lot of work in the winter months, the garage was heated luckily, as summer meant BBQs, gardening, holidays and attending fly-ins so it's harder to find spare weekends. I would also try to plan to do some jobs in the evening even if was only for an hour or so.

I won't go into detail on how I have restored every item as most of the time it's generally the same logic, take photos before stripping the item down, get advice if it's a complex part, when dismantling make notes as months later you will not remember how it was put together and to date there isn't a Haynes manual to help. Inspect it thoroughly, clean and repaint and replace any old nuts, bolts washers etc with new ones. Rebuild and take more photos to keep you motivated. What I will do is list some of the lessons learned that hopefully will be useful to others contemplating a restoration.

Documentation

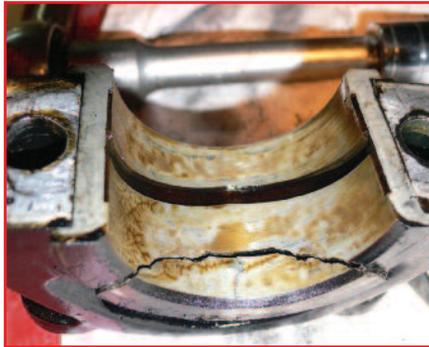
Buy a white board and put it in your workshop, it's useful to put a to-do list on, making notes and writing which parts you need to order. I then backed up the notes on my PC. Acquire the Pilot notes and manual for your model and keep these nearby. Get LAS to send you their Parts Book. Obviously keep all receipts.

Recording the project

Buy some Pipe cleaners, label tie tags, Zip lock bags and permanent marker pens. All are very



The oil filter – 40 years of sludge !!



The Cracked Big End Shell and dried 40 year old oil



The damaged shell can just be seen to the left of the big nut in the middle

useful for labelling and storing parts as you do the strip down. Take photos of everything especially in detail of fittings and the engine area, and then take even more photos as I found there was always one angle I had missed. Don't be in a rush to strip everything down at once, I left my wings until the fuselage was almost back in one piece.

Try to store and stock things together, like all the engine parts in one place, fuselage fixings in another place etc, it makes it easier to locate things later on. An ipad /tablet is useful for keeping documents and photos readily to hand.

Tools

After 4 years I was given a Clarke cabinet tool box on wheels. This is 'the business' as they say and is just over waist high and has a flat top which acts as a good small mobile bench. It's really useful being able to have all the tools, grease, GT86 and so on nearby and somewhere to put things to work on them.

As soon as possible go on to the internet and buy a set of whitworth/BA size spanners and sockets. Metric and UNF will rarely fit and damage the nut

you are trying to free up. Adjustable spanners are ok but sometime difficult to get in small spaces.

Whilst on the internet buy a good magnifying glass to read part numbers and inspect engine surfaces. Ok I was just at the age when I was just starting to need glasses but the 2 magnifiers I bought also have a light and really make it easy to read those small Jxxxx part numbers.

Liquids and Sprays

Don't buy liquid products before you need them as many of them have a limited shelf life.

Marriage Tip: Don't spray underneath the living accommodation (Yes my garage was right under the three bedrooms and although I sprayed the green etch primer outside, the fuselage stank that evening and I was Mr Unpopular for a day or so with the rest of the family).

Apparently WD40 is not good on aluminium, it can corrode it, but GT86 is a good option. And as said by many people, be careful where you spray, fine particles carry in the air, and if you are not protected by a polythene cage then you risk putting a fine layer of paint on whatever is downwind.

Working on the Aircraft and using Elbow Grease

Where it is safe to do so put a bit of heat on seized screws and bolts but be careful where you do it and what is surrounding the culprit. Obviously other well-known tricks like WD40/GT86, turning the nut the opposite way to help free it up and sometimes tapping with a hammer.

After cleaning up steel parts that are not going to be painted, lightly oil or grease them to protect them from rusting again. Have the frame and tail feathers shot blasted and powder coated. I did mine by hand and it was really time consuming. You can ask the company to let you see it before its powder coated so that you can inspect the condition when all the old paint and rust is removed. If in doubt about the condition of the tubing then I believe it's possible to get the whole frame X-rayed as well.

I also made a very useful H frame out of wood to help to turn the fuselage when cleaning, spraying and recovering.



The homemade H frame is really useful for rotating the frame with only one person



The old wooden frame removed from the top of the cockpit.

Pick up Auster spares along the way when they become available, you never know when you might need a spare.

If something isn't going right, don't force it or get frustrated, sleep on it (not literally!), discuss the matter on the forum (ask the audience) or phone a friend. Often going back to it the next day when you're less tired and frustrated magically resolves the issue?

The J/4 isn't yet finished as at the end of last year I moved back to the England and this has effectively delayed the project by 6 months. I am now reverting to a UK permit which means I can keep the original registration. The next steps are to continue to work on finding solutions for bringing the Cirrus Minor 1 back to life, and I will also teach myself

how to use Ceconite 102 as the wings and fuselage are now ready to be recovered and painted.

I guess part 3 of this story will appear in 2-3 years time!

Roger Lane



A test fitting of restored parts always helps to keep morale up.

VT-DGB - AUSTER MK5 c/n 1583

Lycoming 290 with 900 hours airframe & engine

Rishi & Shiel Kumar are hoping to restore to flying condition an Auster that has spent most of its life in India

PRE-OWNERSHIP HISTORY.

Built as Auster MkV TJ530 during March 1945 and issued to 657 Sqn at Gilze Rijen, Holland on 5/4/1945.

After a couple of accidents in Holland and England it was eventually sold to Wiltshire Flying School, Thruxton (possibly in a damaged condition) and registered G-AKJT on 16/12/1947 but C of A not issued until 19-7-48. On 12/7/1951 it was registered to E.W. Kenny, Croydon and then the registration was cancelled 24/3/1952 as 'Transferred to India and registered VT-DGB'.

Registered VT-DGB 24/3/1952 to E.W.Kenny, Borengajuli Tea Estate, Damakuchi, Assam and based on the estate. On the 1/1/1961 it was registered to Tingri Tea Co Ltd, Calcutta and based at Calcutta. It changed hands on 28/3/1967 to M/S Naresh Kumar and Co, 15 Shakespear Sarani, Calcutta.

In the mid 50s many Aircraft & Trucks were auctioned as War Surplus. So these tail draggers with flaps were used by tea company 'Planters' to get in and out of abandoned airfields still in existence at the foothills of the Himalayas, Darjeeling and upper Assam Valleys. A short flight of 15 mins in the hills equalled 2 hours by road.

Pilots were all ex-RAF boys who stayed back to fly 'the hump' in Dakotas and make their fortune. China was an ally then but the mountains were too high to cross. Hence VFR through the valleys was the only way. They did it a thousand times! When the hump dried up in the late 50s, the pilots joined the tea companies with their VFR experience and became bush pilots of sorts. Flight plans were made by phone and morse code! By 1970 all the foreigners had gone.... Garden Owners & pilots too.

Two pilots of repute my father spoke of were, Group Capt. Shorter and Wing Co. Dud Venner.



MORE RECENT HISTORY

About the recent Owners: **Suresh Kumar** (1921- 2010). Soloed in 1942 on Tiger Moth in 4hrs in Alipore flying club, Calcutta. PPL holder. Did Aerobatics in dual. Spoke of performing the Falling Leaf on landing.

Aircraft flown De Havilland Wapiti, Tiger Moth, L1, L5 , Chipmunk , Dragon Rapide, Auster.

Aircraft owned: two L1, L5 (radial) and Auster (Lycoming). As most aircraft on sale had inverted Gypsy major engines, he never bought them as he firmly believed Inverted engines had oiling up issues & could quit mid-flight.

Our family's Auster MK5, VT-DGB, was parked at Dum Dum Airport, Calcutta. Bought from British tea co. Williamson Magor. (Dum Dum is to Calcutta as Heathrow is to London and during WW 2 a squadron of Liberator bombers were based there and in 1968, a tri-weekly London-Calcutta flight operated by BOAC Super VC 10 used the same runway as our Auster!).

VT-DGB was a 3 seater and used as an air ambulance + 1 stretcher - behind the pilot - which went into the rear fuselage. On the left side fuselage behind the pilots door was painted a 15" red cross in a white circle. That was silvered out once we got possession and the stretcher removed to seat 2+2.

Having flown a few times on local circuits and a trip to a nearby beach 125kms away, the Auster was suddenly grounded in 1970 for want of C of A which was unheard of in general flying. It never had a C of A since it was flying in the tea estates which are like bush flying in the outback. To pass the C of A an inspector was appointed who grounded all the aircraft in the flying club hangar.

I remember 2 beautiful Cessna 180s, a Navion and an L5 under restoration. For the Auster the rubber bungees were questioned as she sat noticeably with the left wing low. The bungees were imported and changed by Atherton Engg (AMEs). Next a compression test was ordered on the Lycoming. One cyl psi was a bit lower and on inspection a liner had a light scour mark which was of little consequence. We needed a new liner he said. Lycoming 290c was unavailable, 290d was available said Atherton. The inspector later asked of a 'favour' to let her fly which was refused. And sadly the Auster never flew again!

All grounded/seized aircraft were released to their owners in 1983 with overdue hangar charges which was more than the price of the aircraft. Many

owners disowned their aircraft which were then scrapped and sold in weight. No boneyards here. My father paid up, removed the Auster on a flat bed truck to our factory. It had layers of dust and torn covering. It's log book & some instruments missing! The registration was cancelled from the register on 31/12/1984.

The Auster is currently stored at India's Hobby Centre, Calcutta, and we hope in the future to restore it in the UK (as one owner lives there) and that it will fly again someday.

The aircraft is for sale, serious enquires to Rishi Kumar -email: morsche911@hotmail.com

Thanks to Rishi & Shiel Kumar (and some additional history from Ian O'Neil)



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By Stan Ward



In part one, I described one of the two scares of my stunt-flying career when I misjudged things a bit at Seething.

The other happened at Felixstowe during a Battle of Britain fly-past and display when I was slotted in between an RAF Comet transport and a Phantom – both fairly big irons, one before and one after the little civilian non-radio Auster. I had four minutes to entertain the large crowd collected on the beach and the promenade south of the pier. Clearly, time was important – I had to be around waiting for the Comet to do his stuff while keeping well out of his way, but not far enough to allow the crowd to get bored while I struggled to get into position at 80 mph. Any time lost would have to come out of my four minutes before the Phantom would take over at about 500 mph.

Now all of this is very tricky – without radio you had to watch your watch, watch the Comet in case he over-ran or under-ran his time, watch where you are, then watch your watch and look out for the Phantom while you are tearing the guts out of the aeroplane and yourself in front of the crowd and at low level over the sea.

Being ultra-careful, I normally flew all such shows solo so that the aeroplane was as lightweight as possible, and I did not have to worry about anyone throwing up over me, falling on me or otherwise distracting me, but on this occasion it seemed that to have an assistant with a watch and a pair of eyes would be a positive asset. Bernard Daniel had a feeling for aerobatics and he could count numbers, so he was the partner I selected, and he jumped at the opportunity.

So off we went near to the appointed time heading towards Felixstowe, with four eyes searching the sky trying to spot the Comet in, as they say,

Part 2 of the late Stan Ward's career in display flying

good time: the timing was perfect – it was the flying that caused the trouble, and then only to the two of us. Once again it was the low-level stall turn that caused it – not a gliding one this time, at least it was not intended to be a gliding one because I went into not intended to be a gliding one because I went into it from sea level with full power and lots of speed, but I had misjudged the extra weight of Bernard – normally my aeroplane was very light because there was only me and only just enough fuel to do the show, which often started and finished on an airfield.

But here we were, a bit heavy with Bernard and extra fuel for the return flight and a possible hang-around, so that we lost speed a little more rapidly than usual in the vertically upwards part, with the result that when I ruddered into the yaw which should have put us vertically downwards, we

were going very slowly. So slowly that the change of attitude from upwards to downwards was delayed enough to starve the engine of fuel from the gravity-fed system. That was a pity because the propeller was now slowing down, and I knew that it would not get any encouragement to go faster until either the aeroplane speeded up in the dive or the fuel started flowing again with the change of attitude.

This probably sounds all very technical and unexciting, but it wasn't to us because as the propeller slowed to the point that we could see the individual blades, we were faced with the possibility that it might stop. If we had a self-starter it would have been only a small problem at 500 ft, and the

fact that we did not have a self-starter would have been no problem at all if we had been over an airfield, even at 500 feet. But we were over the sea, and the beach and promenade were well covered with people – beyond them was a row of hotels and funfairs. If we had to come down, it followed that we had only the sea.

Any time lost would have to come out of my four minutes before the Phantom would take over at about 500 mph.

As the propeller slowed to the point that we could see the individual blades, we were faced with the possibility that it might stop

Of course, it didn't happen: don't ask me why not – we hadn't time to pray. Somehow the nose of the aeroplane went down far enough for our speed to increase sufficiently to keep the propeller windmilling until the fuel started to flow again as we pulled out of the dive, and we were back in business. The dry mouths, shaking hands and white faces were not visible to the crowd as we completed our four minutes without further incident.

Generally speaking, display flying is not really dangerous provided that the pilot maintains a firm personal discipline, keeps well in practice, and never does anything that he has not tried out before. Many an item can be made to look dangerous for the effect – for example, the recovery from a spin can be made to look as if there was only a few feet left at the bottom of the recovery dive by delaying the moment and rate of the pull-out.

Personal discipline is required to resist the temptation to add something that has not been thoroughly practised at high level where an accurate assessment of height loss can be made: a sudden desire to add something to an old routine in the belief that, because you are bored with it, the crowd will also be finding it dull, can lead to excessively exciting additions that had not been intended.

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Dates for the diary

SAT 20/SUN 21 JUNE	NORTH WEALD AIR BRITAIN FLY IN phil.kemp@air-britain.co.uk PUBLIC EVENT
SUN 21 JUNE	WELLESBOURNE WINGS AND WHEELS www.xm655.com/events.php PUBLIC EVENT
FRI 26 - SUN 28 JUNE	AUSTER FLY IN & AGM (27th) /MUSEUM TOUR/OLD SARUM LUNCH AT OLD SARUM ON SUNDAY cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS
SAT 4 JULY	ARMY FLYING ASSOCIATION MIDDLE WALLOP FLY-IN cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS
SAT 11/SUN 12 JULY	VALE OF WHITE HORSE GLIDING CLUB FLY-IN Sandhill Farm Swindon cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS
FRI 31 JULY - SUN 2 AUG	EGGESFORD SUMMER CAMP cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS
SAT 15/SUN 16 AUG	OLD TIMER FLY IN SCHAFFEN DIEST (EBDT) BELGIUM guyvalvekens@gmail.com PUBLIC EVENT
SAT 29/SUN 30 AUG	SPANHOE AUSTER CLUB FLY IN cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS
SUN 30 AUG	COLD WAR JETS DAY BRUNTINGTHORPE PUBLIC EVENT
FRI 4 - SUN 6 SEPT	LAA RALLY SYWELL www.sywellaerodrome.co.uk/events.php PUBLIC EVENT
SUN 13 SEPT	MILLER MEMORIAL FLY-IN POPHAM cottys@sky.com IAC MEMBERS FAMILY AND FRIENDS

More events will be posted on the website. PPR form also on website.

Auster J1N Wing Recovering with Oratex 6000

by Paul Ferguson

A few years ago we purchased an Auster that had emerged from Carr farm in pretty good condition since it had been stored in dry storage for around 20 years, before it was brought back to life. After a few years flying her it became evident that the wings needed recovering. This is well illustrated in the above photo of the port wing showing dope through the fabric - time for new fabric. After asking for quotes, the cost associated with paying someone to recover the wings and ailerons was in fact cost prohibitive, when compared to the value of the aircraft. So after some discussion between group members we decided to undertake the restoration work ourselves.

After carrying out some research, the thought of using conventional fabrics and the associated paint systems seemed quite daunting (I am sure those of you that have learned these skills might disagree) so we started to look at the available alternatives. At around this time Robin Aircraft had just launched their new DR401 (an aircraft, which I have to admit that I have admired and wished to own) and they offered as standard a pre-impregnated and pre-coloured fabric that is adhered to the wing structure using easy to use and relatively simple and safe glue system. This meant no paint shop or worries about achieving the perfect painted finish on the fabric.

Since it was being used on certified aircraft I had a pretty sound justification to apply this to a LAA type with the right approvals. I applied to the LAA with the required Mod 2 and Mod 3 forms, including the Oratex application manuals and a technical brief



Trial piece used to practice on before moving on to the wings.

that provided a relatively brief description of the work that I proposed to carry out. Since there were significant health and safety advantages by using the proposed systems, when compared to the other spray paint alternatives, the LAA waived the Mod fee, on the basis that they deemed the proposed approach an innovation related to safety by removing the use of hazardous paint systems. The technical brief proposed that a test assembly would be developed, to trial the proposed techniques, but more importantly to demonstrate the application process to the local inspector.



Port wing removed and on truck. Starboard wing supported to prevent tip over.

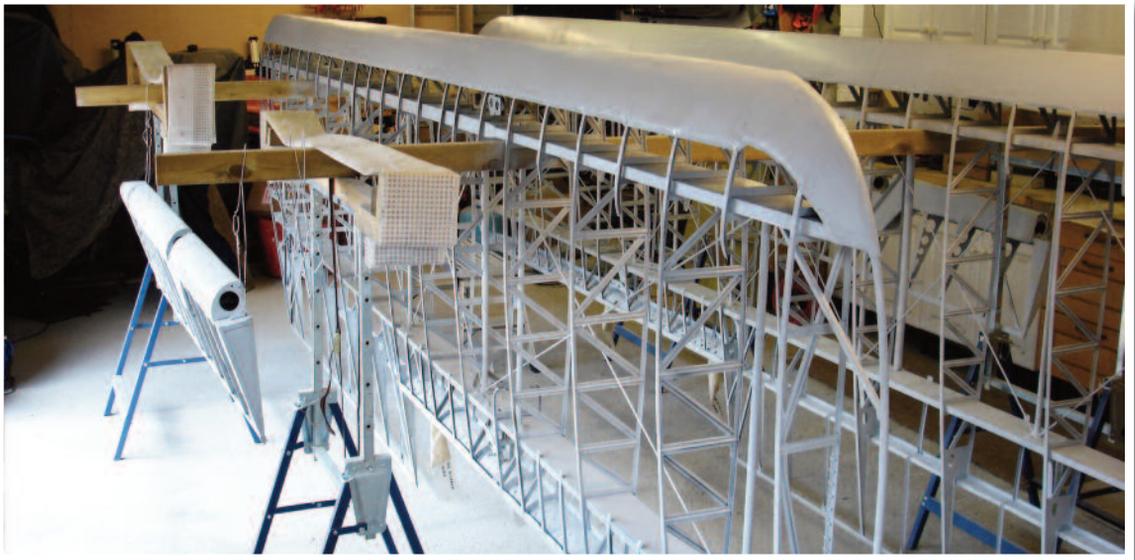
Once the Mod had received the necessary approvals to proceed and a lengthy discussion with my local supportive inspector, we set about removing the wings and transporting them back to my garage/workshop. This process required the assistance of several strong friends and a flat bed truck, but realistically it could have been done with 3 people at a push. Our inspector kindly lent us some rigging pins, which were used to be inserted after each wing bolt is removed to prevent someone having to stand and hold the wing while the bolts are removed in sequence. Although this sounds like a pretty simple idea, I have to admit it was new to me.

After placing the wings on some modified trestles in my garage/workshop we began the process of removing the old fabric. A great tip from our inspector included marking the inspection holes and removing the fabric in a single piece if possible. This would help when applying the new fabric to see how the original application had overcome some of the tricky curves and corners, whilst ensuring that the right inspection holes are located in the right place in the new fabric layer. We also chose to install plastic inspection rings in the new fabric to help simplify future inspections on the wings.



Wing stripped and ready for inspection.

After stripping of the fabric we evaluated the framework of the wings to identify areas that would potentially need some attention before recovering with the new fabric. There was surprisingly very little repair work required, with a small wooden backing plate needing repair along with some small rib repairs being the only major works. At this point our local inspector Rob came back out to visit and inspect the condition of the wing. We agreed the minor fixes and he proposed if we had the time and inclination we could rub down all the surfaces, apply Zinc Primer to the bare parts (most of the rubbed down surfaces), allowed to dry and then applied a couple of coats of two pack primer to provide a long term protection that is inert to glues and solvents under the newly applied fabric. At this point the ribs were repaired with replacement of loose rivets and the controls pulleys were serviced. In parallel the ailerons were undergoing the same treatment as the wings, whilst the flaps were stripped of paint and prepped for reconditioning.



Primed metal surfaces ready for Fabric

Glue was applied to all of the surfaces that were to be used to adhere the fabric to the wings and the same glue applied to the mating fabric side and allowed to air dry. The fabric is then laid over the wing and the glue is activated by using a hot air gun and iron.

At the request of the inspector the technique was first applied on an aileron to demonstrate its effectiveness and the skill of the applicator and after inspection, approval was received to proceed with applying the fabric to the wings.

Once glued and left for a few days to settle the fabric can be tightened using an air gun or iron and then prepared for stitching. The application manual

allows this heat shrinking immediately after gluing but we found that better results were achieved by allowing the glue to go off before shrinking. The stitching technique used was the same as the technique used for other fabric. The fabric was applied to one side of the wing then inspected, prior to the application of the mating side thus completely covering the wing. This allowed the inspector to validate that both the procedure and techniques used were adequate

After adding the inspection patches the wings were ready for the final inspection and application of the wing lettering using a very easy to apply transfer.

Wings finished and lettering applied



After completing the wings it was clear that the high quality of the finish was really going to contrast against the fuselage so, as these projects usually go I am informed, this one grew in size and I found the fuselage in my garage/workshop for a bit of a touch up. This, being less exciting, is not discussed at length here but I have included a picture of the nearly finished product below.

The fuselage and wings are due back to the airfield soon and after test flight and final approval from the LAA will be back in service in time for the summer.

The family gets involved with the dismantling at Spanhoe



Fuselage after a blow over with dope paint and markings reapplied

Reasons for choosing Oratex 6000. The main reasons for choosing this system were...

- The ease of application.
- It is a very easy system to use and is a lot quicker than the more traditional systems.
- The manufacturers claim there are weight savings and speed increases (none of which were drivers for me in this application).
- Ease of maintenance as the surface is remarkably easy to clean, which should help with bug removal and hangar dust cleaning.

- Ease of repair. With spare material left over all you need to do to facilitate a repair is make a patch of suitable size and glue on and heat with gun. If there is no puncture it is possible to re heat shrink the fabric to repair stretches.

At the time of writing this Oratex Aircraft Fabric was awarded the "Innovation of the Year" for this fabric at the Aero Expo as part of the German Fliegermagazin Awards 2015.

MEMBERS SALES & WANTS

If members have any sales or wants please send your details to editor@austerclub.org

FOR SALE: PROPELLER suitable for Gipsy engined Austers (or Tiger Moths)

Also many other Auster spares available call Carl Tyers 01780 450205.

WANTED: CIRRUS MINOR 1 for my Taylorcraft Plus D. Whole engine or serviceable crankcase. Have many engine spares to trade/swop for right item. Please phone Adrian on 07860-667807.

FOR SALE: Blackburn Cirrus Minor 2 or 2A Impulse Magneto SG4-16. Unused since rebuild by Hants&Sussex Aviation now (Deltair Airmotive Ltd) £600.00p - Ron Neal Tel 0116 260 6092.

FOR SALE: Gipsy major 10 exhaust system with heater tube almost as new condition complete. 11.5 Gallon fuel tank gauges x 2. A.S.I in knots. Horizon very good condition. Sparkplugs some with very little use 12mill screened type. R.P.M. Angle drives. Tel 07966621401.

FOR SALE: My manuals library[Auster/Gipsy/Cirrus] is for sale, as is a Gipsy Major engine transit stand plus a few more Auster/Gipsy related items. Hugh Jones - hugh@swantonabbott.com

FOR SALE: Auster J/5F Aiglet Trainer G-AMZT

S/N3107- Built 1953 for College of Aeronautics.

Same owners since 1980. Currently hangared and operated from White Waltham. T/T Airframe 5,300 hrs. Engine Gipsy Major 1 (electric starter). Time since O/H 1,850 hrs. Time since major rebuild 1,140 hrs. Time since last top overhaul 200 hrs. Maintained to LAMS. Permit to Fly renewed until May 2016. £18,000 Contact: David Hyde 01344 842602 , email: david.hyde13@btinternet.com John Saull 01342 822712 email john@ifairworthy.fsnet.co.uk



FOR SALE: G-ASMZ VF516 Auster A61 Terrier 2

Engine Gipsy Major 10 - 2 35hrs since complete overhaul.

Ceconite Covering. LAA Permit to Fly 10-05-15. Royal Air Force Exemption. Radio VHF COM Narco. Full Histry of VF516 Korean Conflict etc.

£25000 For Info: 01237 471908 austert2@brynandrews.com



FOR SALE: G-ARHM VF557 Auster AOP 6

Engine Gipsy Major 10 - New LAA Permit

Mode S Transponder

Call: 01780 450 205 for details



FOR SALE: G-ANHX TW519 Auster 5D

Engine Gipsy Major 1 - zero houred by Deltair.

Recently restored and with new LAA permit.

For full history and details contact

David Baker at Auster Spares 01522 778617



FOR SALE: Beagle A109 Airedale G-ASRK

180hp Lycoming. Total of 10 hrs so far since Notice 75 on the prop. November 2012 – New LAA permit Nov 2014.

Professionally maintained. Good compressions and good all round performance. Long range tanks fitted.

Total A/F 1521hrs. Engine top overhaul at 1600hrs, total time to date 1895hrs. VP Prop 16.40hrs since overhaul

All A/Ds complied with - Logs up to date and available for inspection. 4 place intercom. Original operation

manuals Parts manuals and full history, 3 headsets, cabinet full of consumables – Oil etc, Spats (not Fitted). Full covers for wings and cockpit. Reasons for sale - no time to really make use of the aircraft and change in business circumstances. If you want to fly it - £80.00 per hour in advance - refundable if you purchase.

Great aircraft for touring and hours building

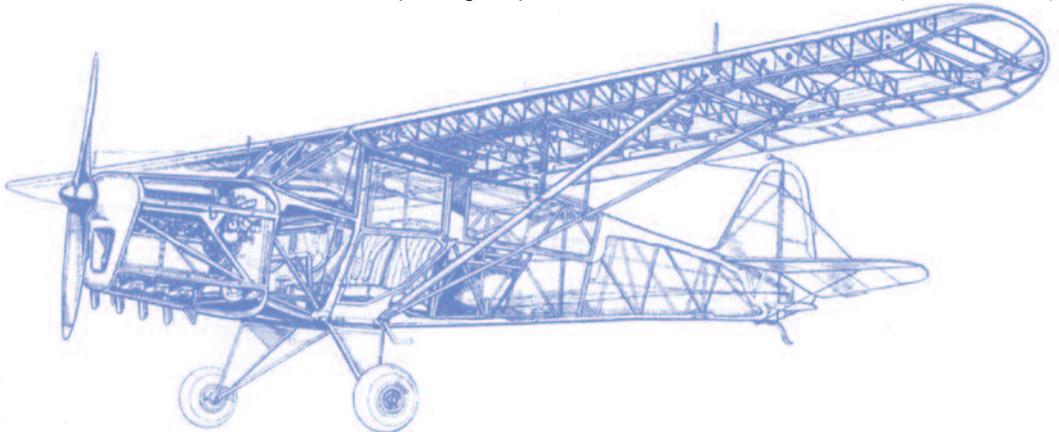
Open to sensible offers on £15,000 Call - 01233 636678 day

01233503975 eve. - 0776 542 4886 Mob.



LIST OF USEFUL CONTACTS (UK based unless stated)

- Adams Aviation** www.adamsaviation.com Suppliers of aircraft parts and maintenance items
- Aircraft Spruce** www.aircraftspruce.com Wide range of parts for all types of aircraft
(US based - agents worldwide - LAS Aero in UK)
- Airedale aviation** airedale@ispmedia.co.uk Specialising in graphics for the aviation industry
- Auster Spares** **Dave Baker** Tel/fax +44 (0)1522 778617 Wide range of original and second hand Auster spares
- Bex Aviation** **Rebecca Tyers** 01780 450 205 Aircraft recovering
- Burlen** www.burlen.co.uk Spares for AMAL fuel pumps
- Cambrai Covers** www.cambraicovers.aero Aircraft covers and ground support equipment
- CKT Aero Engineering** www.cktengineering.com Aircraft exhausts specialists
- Desser Tires** www.desser.com Wide range of tires and tubes (USA based)
- Deltair Airmotive Ltd** www.deltair.co.uk Specialises in aircraft engine and propeller overhaul or repair
- Haire Aviation** **google Haire Aviation for link to ebay** Screws, Nuts and Bolts and fasteners. (USA based)
- Hercules Propellers** www.hercprops.com Custom produced propeller manufacture and refurbishment
- Ferodo Brakes** www.ferodo.co.uk Can re-line Auster brake shoes
- LAS Aero** www.lasaero.com Wide selection of engine and airframe parts and consumables
- LP Aero Plastics** www.lpaeroplastics.com Can manufacture canopies/windcreens using original moulds (USA based)
(Patrick Harrington www.vintaero@bigpond.net.au has licence in Australia from LP Aero Plastics to make canopies)
- LX Avionics** www.lxavionics.co.uk Light aircraft avionics supply
- Ribblesdale Auto Electrics (Units) Ltd** www.magneto.co.uk/rewinding.html Magneto servicing/overhaul and rewinding of armatures
- Simply Bearings** www.simplybearings.co.uk Wheel bearings and magneto drive shaft bearings
- Skycraft Services Ltd** www.skycraftservices.co.uk Hoffman propellers distributor. Sales and service.
- Skydemon** www.skydemon.aero Flight planning and navigation software for iPad, Android and PC
- Springs** www.assocspring.co.uk Variety of springs
- Swindon Aircraft Timber Company** www.aircraftplywoodandtimber.co.uk Aircraft wood such as Spruce and Douglas fir
- Transair** www.transair.co.uk Wide variety of equipment for aircraft and pilots
- Wag Aero** www.wagaero.com Various aircraft parts including Taylorcraft
- Watts Aviation** www.wattsgroup.co.uk Specialist distributor of aircraft tyres
- Windmill Aviation** **Carl Tyers** 01780 450 205 Auster maintenance and spares and advice on all things Auster
- Wind Driven Generators** **John Pearson (Nottingham)** 0115 970 4439/0775 269 0198 Wind driven generator servicing



Just A Sec....

As you know we strive to provide a varied range of events for you to choose which you prefer to attend. There are however two events in the forthcoming calendar which I personally and the Committee ask you to attend. Think of it as a call to arms!

The first is the **AGM on June 27th**, which is at Old Sarum. As those of you who have attended the AGM for the last couple of years will know, the Committee put a lot of time and effort into making these events more than a meeting. We aim for the content to be informative, interesting and relevant.

Friday 26th	Optional arrival day and camp over
Saturday 27th	13:30 Tea, coffee and biscuits 14:00 AGM (and raffle of a Sky Demon Subscription – donated by Sky Demon) 14:50 Group guided tour of the aircraft collection
Saturday night	Camp over
Sunday 28th	11:30 Fly-out to Compton Abbas for lunch Depart from home

We have negotiated: **Free landing - Free camping next to aircraft and overnight use of a toilet**

The second is the **LAA Rally at Sywell on 4th to 6th September**, where we will have a stand in Marque 3. The Rally is unquestionably the largest gathering of GA pilots and activity in the UK and whilst it was once very well supported by our members this has subsided over recent years.

With one of the Club's goals being to promote the Auster and the Club within the GA community it would be great to see a good solid line of Austers parked, proudly representing their heritage.

For both events please let me know if you intend to attend. I know actual attendance is dependent upon; weather, technical issues and other numerous variables but it is very useful to have an indication numbers which helps with planning.

Membership Report

Membership currently stands at 272 and is slowly edging back towards the three hundred mark that we aim at. The club is still a truly International organisation with the distribution of members as follows,

Australia - 25	Austria - 2
Belguim - 2	Canada - 1
Denmark - 2	Great Britain - 197
Finland - 2	France - 6
Germany - 2	Iceland - 1
Luxembourg - 1	Netherlands - 1
New Zealand - 11	Northern Ireland - 2
Norway - 2	Portugal - 2
Ireland - 4	South Africa - 1
Sweden - 3	Switzerland - 2
UAE - 1	USA - 1

We welcome the following new members to the club and of special interest we now have members based in India (see article on VT-DGB – thanks Rishi for helping to put it together).

David Jones (Airspeed Aviation) (England)
Les ailes d'autres fois (Loic Domain -France)
David Hill (England)
Michael Draper (England)
Rishi and Shiel Kumar (India)
Paul Bates (England)
Colin Harding (England)

Facebook attracted 100 more followers with 572 people now following us.

Roger



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Auster Drawing & Technical Archive:

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Enquiries should be directed to Peter Stoddart
Tel: 0116 277 5932

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Join the IAC

IAC annual membership fees:

- £24 per year for people paying by Standing Order (preferred method)
- £27 per year if paid by Paypal, cheque, debit/credit card or internet bank transfer.
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Auster Instructors

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G�rard Caubergs	Belgium	0032 474 52 64 16	gee@scarlet.be

Jon Cooke, Stephen Briggs, Alan Cooper, Mike Cuming, Tony Davis, Peter Gill, Robin Helliar-Symons, Marc Le Gresley, Bob Moore, Steve Copeland, and Arnaud Bazin are all authorised Flight Examiners, who can sign a Revalidation Certificate and/or fly with someone who needs a Licence Skill Test (because their licence was not revalidated before the appropriate date). Any of the above will be happy to carry out the biannual instructor flight, JAR difference training or type conversion on Austers. Please contact your local instructor for local arrangements and please also remember that acquiring and maintaining instructor qualifications is not cheap and they will have to ask you to pay a reasonable amount for their time and travelling expenses. Any instructors interested in going on the list are welcome to contact Robin Helliar-Symons, email RDHSymons@gmail.com

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